



Plan of Construction Operations

REHABILITATION OF APRON I

At

Prince George Airport, BC

Tetra Tech Project No. TRN.AIRP03038

Date: May 20, 2016

Site Name: Prince George Airport

Project: Rehabilitation of Apron I

Start Date: June 2016 Finish Date: July 2016

Originator: Name: Cuyler Green

Company Prince George Airport Authority

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Description of Work

The project consists of cold milling and paving new asphalt, rehabilitate existing underground storm pipes and new paint markings. Provisional Work may go ahead for constructing new PCC concrete aircraft parking panels. Construction work is confined to Apron I.

All work on airside will conform to TP312E Transport Canada's *Aerodrome Standards and Recommended Practices* manual 4th Edition.

Work Sections / Operational Constraints

The work on this project will involve airside work. The work sections below identify the work in each section, see also the Plan of Construction Operations drawing(s).

WORK PROGRAM

The work program and construction sections were developed to minimize the impact and restrictions on airport operations. The specific project limits for each work section are shown on the Plan of Construction Operations (PCO) drawing(s) (attached).

The construction has been separated into three separate sections with operation requirements, work restrictions and airport operational changes provided for each section. Following is a brief description of the scope of work for each Work Section, the PCO drawing(s) shall be read for the requirements.

Work Section A- Daytime (Provisional Work):

Scope of Work:

Secure 1.8m high temporary fencing with steady burning red lights to be installed in Section A for new PCC concrete panel construction. Remove existing asphalt and gravels to new subgrade elevations, including excavation, construction of new concrete panels, new asphalt paving, and paint markings.

Work Section B (Phase A and B)- Daytime:

Scope of Work:

Remove existing asphalt by cold milling, new asphalt paving and paint markings on Apron I. This work will be done in two phases in order to minimize impact to aircraft operational stand parking.

Work Section C – Nighttime:

Scope of Work:

Remove existing asphalt by cold milling, new asphalt paving and paint markings on Apron I. Storm pipe underground rehabilitation between manholes 1 – 7.

Stages/Phase/Areas of the Construction & Schedules:

Project Milestones

The Following Milestones are envisioned in the preparation and execution of the Rehabilitation of Apron I project:

- Commence Construction: Late June 2016
- Expected Completion Date: July 2016

Proposed Work Plan

Construction traffic will be separated from the air traffic as described above.

Types & Frequency of Air Traffic:

See Annex 3 for Flight Schedule.

Disruptions to Air Traffic:

Partial closure of Taxiway D between west end of Apron II and Apron I, closure of Taxiway C, partial/total closure of Apron I. All closures dependant on the work Section being constructed.

Position and height of equipment (Relative to Runways & Taxiways):

Types of Equipment:

(Note: All heights are above ground level)

- Standard Dump Truck (approx. 13 ft.) (4m)
- Access Vehicles (pick-up trucks)
- Paving Equipment (approx. 12 ft.) (3.7m)
- Milling machine (approx. 12 ft.) (4m)

- Roller (approx. 12 ft.) (4m)
- Booms down; Excavator (approx. 18 ft) (5.5m) or mini excavator (approx. 12 ft.) (3.7m)

Work Adjacent to Taxiways / Aprons:

Work on Apron I and adjacent to Taxiway 'C' and 'D' and Apron I will be done in accordance with TP312E - Aerodrome Standards & Recommended Practices, 4th Edition with operational constraints as described previously.

Unserviceability Delineators Provided:

Unserviceability delineators will be placed as detailed on the drawings and the attached Annex 5.

Displaced and/or Relocated Thresholds:

None.

NOTAM:

1. All NOTAMs issued for the project shall be issued in accordance with NAV CANADA's "Canadian NOTAM Procedures Manual".
2. Below is a sample NOTAM to reflect the current work schedule. The schedule may be adjusted to increase or decrease the closures depending on work progress.

CYXS
TWY D CLSD BTWN WEST END APRON II AND APRON I DUE
CONSTR.
TWY C CLSD DUE CONSTR.

1607111400 TIL APRX 1607240100

Access Control, Vehicle Operations and Airside Escort:

Site Access

Access will be through an existing airside gate south of the RCMP Hangar as shown on the drawing(s). If Provisional Work for the new PCC panels is constructed, a separate access through an existing airside gate onto Taxiway D / Apron II will be utilized for this work.

See below for details of vehicle, personnel and access control requirements including access control guards and the Airside Escort.

Airside Escort

Control over construction men and equipment and the work crews will be provided by the Airside Escort. This person(s) fully comprehends Prince George Airport's airside safety procedures and regulations and shall issue and recover temporary security passes. The Airside Escort(s) shall be accessed through the Prince George Airport's Security Office.

The Airside Escort will control the activities of the Contractor's men and equipment and the inspection staff by rigidly enforcing the airside regulations.

The Airside Escort will continuously monitor air and ground traffic frequencies as required for the work activities.

The Airside Escort must meet the following requirements:

Possess an Airside Vehicle Operator's Permit for The Prince George Airport and a valid Restricted Radio Operator's Licence.

Airside Escort Vehicle

The airside escort vehicle shall be equipped with a 360 degree flashing amber beacon, a 2-way radio and a VHF radio capable of receiving and transmitting on the airport frequencies. This vehicle will require an airside vehicle permit. The operator of the vehicle must possess the same qualifications as the Airside Escort. The Airside Escort Vehicle shall be provided by the Airport.

Access Control Guard (Gate Guard)

The Access Control Guard(s) must fully comprehend Prince George Airport's airside safety procedures and regulations.

The Access Control Guard will control access to the construction site. The Contractor will provide the personnel for this position. The Airport will provide training for the Contractor's personnel for this position.

Temporary Airside Passes

All Contractor's personnel are required to have a temporary airside pass for all work on airside and shall receive and return the passes to the Airside

Escort as arranged by the Airport's Security Office. A charge of \$200.00 to the Contractor will be levied for each pass not returned.

Movement Restrictions

Movement of Contractor's personnel and equipment is restricted to construction areas. See Plan of Construction Operations drawing in this document.

Vehicle Control Requirement

- .1 Airside Escort shall control all Contractor's personnel and equipment when on airside.
- .2 Airside Escort shall escort all Contractor's personnel and equipment to and from the work site.
- .3 Contractor's personnel must obey directions given by the Airside Escort at all times without delay. ***Contractor personnel who disregard directions given to them by the Airside Escort will be subject to immediate removal from the site for the duration of the project.***

Communications Plan (Prior to Construction & During Construction):

See attached Lines of Communication Chart in Annex 2.

1. RESPONSIBILITIES

The following are the basic responsibilities of those directly involved in the project, during construction:

.1 DIRECTOR OF OPERATIONS

The Director of Operations or designated representative is responsible for the following:

- .1 Advising aircraft operators of the construction schedule updates and operating procedures for the various Phase/Areas of work.
- .2 Issuing NOTAMs advising of operational constraints including closure of Taxiways at various times during the project.
- .3 Advising the Resident Engineer of any operational, safety or security concerns that arise during the project.
- .4 Advising NAV Canada of changes in the construction schedule which might impact on their primary responsibilities.

- .5 Participating in the acceptance of the completed work as the operational representative.
- .6 Meeting with Contractor and/or Resident Engineer as required to inspect any surfaces being reopened and to review operational, safety, security concerns and schedule for the following work shift.
- .7 Designating a safety reviewer who will make periodic inspections of the entire job site and who will review in detail:
 - F.O.D. (foreign object damage) prevention,
 - security
 - safety.
- .8 Advise Transport Canada of project completion.

.2 NAV CANADA

- .1 Ensuring operational directives issued by the Director of Operations are followed in accordance with Air Traffic Control (ATC)/Flight Service Station (FSS) operational procedures.
- .2 Advising air traffic of NOTAMs and voice advisories that are in effect.
- .3 Advising air traffic of operational restrictions and limitations regarding airport facilities.

.3 RESIDENT ENGINEER

The Resident Engineer will liaise with the Director of Operations or designated representative and is responsible for the following:

- .1 Co-ordinating the construction work through the Contractor.
- .2 Advising the Director of Operations of any problems, safety or security concerns related to the project.
- .3 Ensuring that operational directions provided by the Director of Operations are followed by all workers.
- .4 Ensuring the Airport is advised 72 hours prior for any approved NOTAM changes so that the Airport may advise NAV Canada – ATC/FSS Manager.
- .5 Ensuring that the Contractor complies with all airport safety, security, and operational requirements for the project at all times.
- .6 Monitoring construction progress, quantity and quality of construction materials and inspection performance.

- .7 Implementing systems and procedures to ensure proper inspection and testing of the work.
- .8 Responsible for all other aspects of the construction project not specifically assigned.

.4 CONTRACTOR

The Contractor is responsible for the construction work on the project as specified in the contract. The following requirements for the Contractor are mandatory and will be incorporated into the contract documents.

- .1 Contractor's personnel and subcontractors are restricted to the area perimeters shown on the PCO drawing(s).
- .2 Security regulations are to be followed.
- .3 Vehicular traffic regulations are to be followed.
- .4 Directions from the Airside Escort are to be followed.
- .5 A list of reliable employees that will require temporary airside passes is to be provided. Valid photo ID, acceptable to the Airport, is required by all personnel requiring a temporary pass.
- .6 All private vehicles, materials, and non-working equipment are to be located on Groundside.
- .7 Manoeuvring areas and public use areas to be kept clean and free of dust and debris.
- .8 All Contractor's vehicles permitted on airside as part of the work requirements must be equipped with a 360 degree flashing amber beacon.
- .9 Contractor to provide and/or install all necessary barricades, lights, delineators as noted on the drawings and/or directed by the Airport at no cost to the Airport.
- .10 Contractor shall provide the personnel for the position(s) of Access Control Guard.

2. LINES OF COMMUNICATION

The following is a summary of communication procedures relating to the project implementation stage (see Annex 2). Only the Director of Operations or designate shall have communication with Transport Canada or NavCanada.

.1 DIRECTOR OF OPERATIONS

The Director of Operations will advise tenants and air operators of operational status of facilities and any scheduled interruptions.

Requirements and concerns of tenants and air operators will be communicated to the Resident Engineer.

The Director of Operations will liaise with the Resident Engineer and Manager ATC/FSS to resolve operational or security concerns.

.2 RESIDENT ENGINEER

The Resident Engineer will respond to operational or safety concerns made known by the Director of Operations and will direct the Contractor. The Resident Engineer will liaise with the Director of Operations.

The Resident Engineer will advise the Director of Operations of proposed changes to the work.

The Resident Engineer is to respond to operational concerns made known to him by the Director of Operations. He will direct the Contractor accordingly. If there are problems he cannot resolve, he is to advise the Director of Operations.

The Resident Engineer will issue a shift summary to YXS Operations staff and Project Managers outlining shift activities, construction progress, irregularities, and upcoming shift activities and any operational changes required.

.3 CONTRACTOR

Safety and security are paramount at the airport site. The Contractor will assume full responsibility for all construction workers, including Sub-Contractors and advise them accordingly.

.4 AIRLINE OPERATORS, TENANTS, AND MAJOR USERS

All airline operators, tenants and major users are to make their operational concerns known to the Director of Operations.

.5 REPORTING SYSTEMS

.1 SITE INSTRUCTIONS

The Director of Operations is to issue site instructions to the Resident Engineer verbally and confirmed in writing, to revise safety or operations procedures as required.

Security and Safety

All persons, equipment and vehicles permitted airside of the security fence by virtue of doing work under this project must remain within the delineated boundaries of the work area and access route. No private vehicles will be permitted airside of the security fence. No person will be permitted airside except during approved scheduled working hours without the specific authorization of the Director of Operations.

1. Security

Responsible Personnel

Provide the Director of Operations with a list of Consultant and Contractor's responsible personnel who may be contacted after working hours in case of emergency.

2. Safety

YXS will conduct a safety case with Airport Operations and stakeholders in advance of the work.

YXS and /or the Resident Engineer will conduct a face-to-face briefing with the Contractor at the start of the work and ongoing at regular intervals throughout construction as a reminder to ongoing employees and to ensure that any new employees are briefed. It is the responsibility of the Contractor to ensure all personnel on site have been briefed on airside safety procedures.

Safety is a regular agenda item at construction meetings.

All vehicles operating Airside of the Prince George Airport must be equipped with a 360 degree flashing amber beacon. This beacon must be on when Airside.

All workers on airside shall wear high visibility clothing and personal protection equipment as required by WorkSafe BC.

The safety of aircraft, passengers, and the public must not be compromised during this project. Any operations that may jeopardize the safety of operations must be reported immediately to the Director of Operations and Resident Engineer.

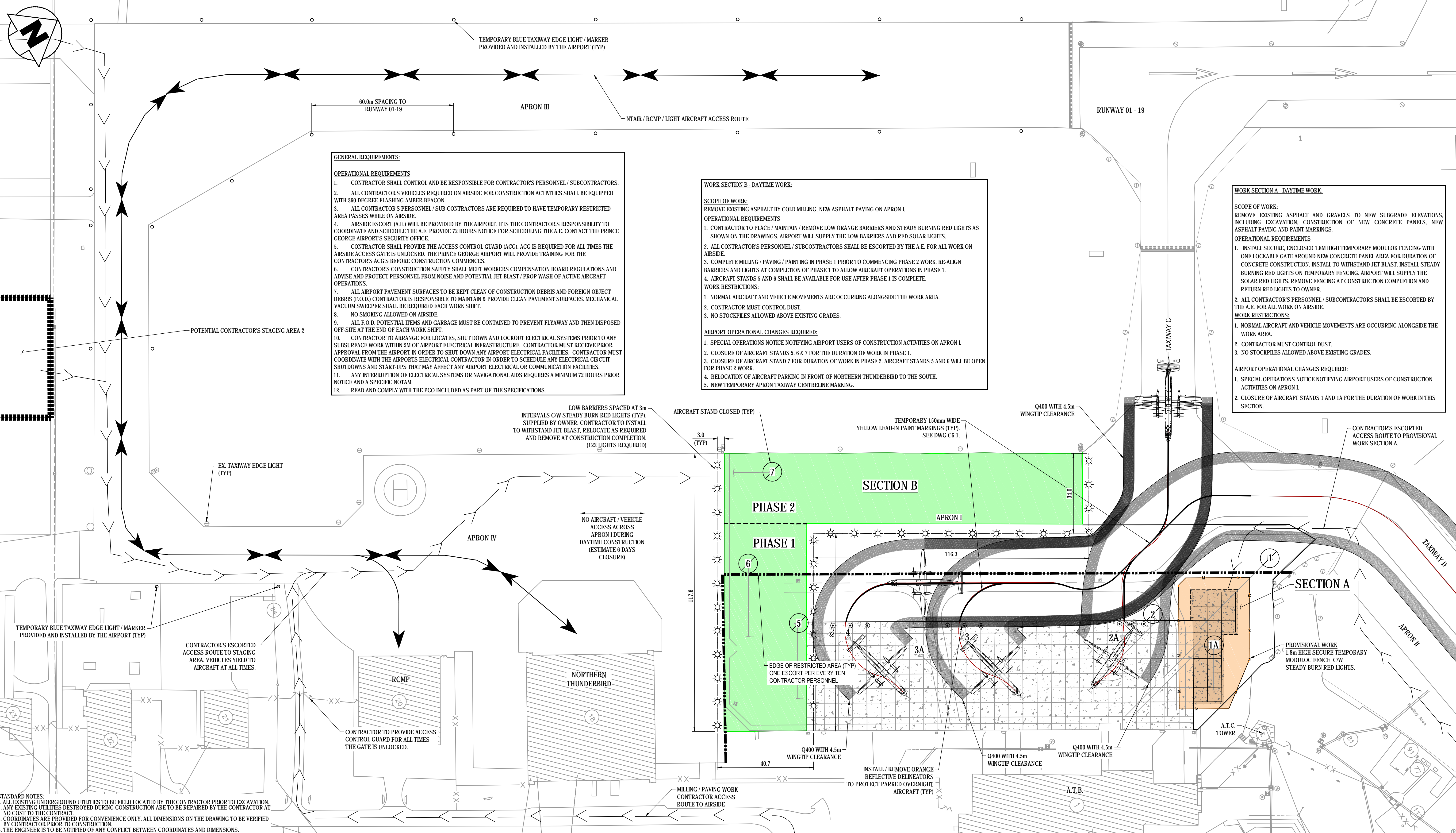
A report shall be made to Airport Operations by telephone or radio at the end of each work shift by the Resident Engineer to confirm that the surfaces adjacent to the work area have been inspected by himself, the Contractor and the Director of Operation's representative and is in suitable condition for aircraft operations.

The Director of Operations has the authority to issue a "Stop Work Order".

Annex 1

Plan of Construction Operations

See Drawing C2.0 and C2.1



GENERAL REQUIREMENTS:

OPERATIONAL REQUIREMENTS

- CONTRACTOR SHALL CONTROL AND BE RESPONSIBLE FOR CONTRACTOR'S PERSONNEL / SUBCONTRACTORS.
- ALL CONTRACTOR'S VEHICLES REQUIRED ON AIRSIDE FOR CONSTRUCTION ACTIVITIES SHALL BE EQUIPPED WITH 360 DEGREE FLASHING AMBER BEACON.
- ALL CONTRACTOR'S PERSONNEL / SUB-CONTRACTORS ARE REQUIRED TO HAVE TEMPORARY RESTRICTED AREA PASSES WHILE ON AIRSIDE.
- AIRSIDE ESCORT (A.E.) WILL BE PROVIDED BY THE AIRPORT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE AND SCHEDULE THE A.E. PROVIDE 72 HOURS NOTICE FOR SCHEDULING THE A.E. CONTACT THE PRINCE GEORGE AIRPORT'S SECURITY OFFICE.
- CONTRACTOR SHALL PROVIDE THE ACCESS CONTROL GUARD (ACG). ACG IS REQUIRED FOR ALL TIMES THE AIRSIDE ACCESS GATE IS UNLOCKED. THE PRINCE GEORGE AIRPORT WILL PROVIDE TRAINING FOR THE CONTRACTOR'S ACG'S BEFORE CONSTRUCTION COMMENCES.
- CONTRACTOR'S CONSTRUCTION SAFETY SHALL MEET WORKERS COMPENSATION BOARD REGULATIONS AND ADVISE AND PROTECT PERSONNEL FROM NOISE AND POTENTIAL JET BLAST / PROP WASH OF ACTIVE AIRCRAFT OPERATIONS.
- ALL AIRPORT PAVEMENT SURFACES TO BE KEPT CLEAN OF CONSTRUCTION DEBRIS AND FOREIGN OBJECT DEBRIS (F.O.D.) CONTRACTOR IS RESPONSIBLE TO MAINTAIN & PROVIDE CLEAN PAVEMENT SURFACES. MECHANICAL VACUUM SWEEPER SHALL BE REQUIRED EACH WORK SHIFT.
- NO SMOKING ALLOWED ON AIRSIDE.
- ALL F.O.D. POTENTIAL ITEMS AND GARBAGE MUST BE CONTAINED TO PREVENT FLYAWAY AND THEN DISPOSED OFF-SITE AT THE END OF EACH WORK SHIFT.
- CONTRACTOR TO ARRANGE FOR LOCATES, SHUT DOWN AND LOCKOUT ELECTRICAL SYSTEMS PRIOR TO ANY SUBSURFACE WORK WITHIN 5M OF AIRPORT ELECTRICAL INFRASTRUCTURE. CONTRACTOR MUST RECEIVE PRIOR APPROVAL FROM THE AIRPORT IN ORDER TO SHUT DOWN ANY AIRPORT ELECTRICAL FACILITIES. CONTRACTOR MUST COORDINATE WITH THE AIRPORT'S ELECTRICAL CONTRACTOR IN ORDER TO SCHEDULE ANY ELECTRICAL CIRCUIT SHUTDOWNS AND START-UPS THAT MAY AFFECT ANY AIRPORT ELECTRICAL OR COMMUNICATION FACILITIES.
- ANY INTERRUPTION OF ELECTRICAL SYSTEMS OR NAVIGATIONAL AIDS REQUIRES A MINIMUM 72 HOURS PRIOR NOTICE AND A SPECIFIC NOTAM.
- READ AND COMPLY WITH THE PCO INCLUDED AS PART OF THE SPECIFICATIONS.

WORK SECTION B - DAYTIME WORK:

SCOPE OF WORK:
REMOVE EXISTING ASPHALT BY COLD MILLING, NEW ASPHALT PAVING ON APRON I

OPERATIONAL REQUIREMENTS

- CONTRACTOR TO PLACE / MAINTAIN / REMOVE LOW ORANGE BARRIERS AND STEADY BURNING RED LIGHTS AS SHOWN ON THE DRAWINGS. AIRPORT WILL SUPPLY THE LOW BARRIERS AND RED SOLAR LIGHTS.
- ALL CONTRACTOR'S PERSONNEL / SUBCONTRACTORS SHALL BE ESCORTED BY THE A.E. FOR ALL WORK ON AIRSIDE.
- COMPLETE MILLING / PAVING / PAINTING IN PHASE 1 PRIOR TO COMMENCING PHASE 2 WORK. RE-ALIGN BARRIERS AND LIGHTS AT COMPLETION OF PHASE 1 TO ALLOW AIRCRAFT OPERATIONS IN PHASE 1.
- AIRCRAFT STANDS 5 AND 6 SHALL BE AVAILABLE FOR USE AFTER PHASE 1 IS COMPLETE.

WORK RESTRICTIONS:

- NORMAL AIRCRAFT AND VEHICLE MOVEMENTS ARE OCCURRING ALONGSIDE THE WORK AREA.
- CONTRACTOR MUST CONTROL DUST.
- NO STOCKPILES ALLOWED ABOVE EXISTING GRADES.

AIRCRAFT OPERATIONAL CHANGES REQUIRED:

- SPECIAL OPERATIONS NOTICE NOTIFYING AIRPORT USERS OF CONSTRUCTION ACTIVITIES ON APRON I
- CLOSURE OF AIRCRAFT STANDS 5, 6 & 7 FOR THE DURATION OF WORK IN PHASE 1
- CLOSURE OF AIRCRAFT STAND 7 FOR DURATION OF WORK IN PHASE 2. AIRCRAFT STANDS 5 AND 6 WILL BE OPEN FOR PHASE 2 WORK.
- RELOCATION OF AIRCRAFT PARKING IN FRONT OF NORTHERN THUNDERBIRD TO THE SOUTH.
- NEW TEMPORARY APRON TAXIWAY CENTRELINE MARKING.

WORK SECTION A - DAYTIME WORK:

SCOPE OF WORK:
REMOVE EXISTING ASPHALT AND GRAVELS TO NEW SUBGRADE ELEVATIONS, INCLUDING EXCAVATION, CONSTRUCTION OF NEW CONCRETE PANELS, NEW ASPHALT PAVING AND PAINT MARKINGS.

OPERATIONAL REQUIREMENTS

- INSTALL SECURE, ENCLOSED 1.8M HIGH TEMPORARY MODULOK FENCING WITH ONE LOCKABLE GATE AROUND NEW CONCRETE PANEL AREA FOR DURATION OF CONSTRUCTION. INSTALL TO WITHSTAND JET BLAST. INSTALL STEADY BURNING RED LIGHTS ON TEMPORARY FENCING. AIRPORT WILL SUPPLY THE SOLAR RED LIGHTS. REMOVE FENCING AT CONSTRUCTION COMPLETION AND RETURN RED LIGHTS TO OWNER.
- ALL CONTRACTOR'S PERSONNEL / SUBCONTRACTORS SHALL BE ESCORTED BY THE A.E. FOR ALL WORK ON AIRSIDE.
- NO STOCKPILES ALLOWED ABOVE EXISTING GRADES.

WORK RESTRICTIONS:

- NORMAL AIRCRAFT AND VEHICLE MOVEMENTS ARE OCCURRING ALONGSIDE THE WORK AREA.
- CONTRACTOR MUST CONTROL DUST.
- NO STOCKPILES ALLOWED ABOVE EXISTING GRADES.

AIRCRAFT OPERATIONAL CHANGES REQUIRED:

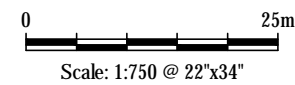
- SPECIAL OPERATIONS NOTICE NOTIFYING AIRPORT USERS OF CONSTRUCTION ACTIVITIES ON APRON I
- CLOSURE OF AIRCRAFT STANDS 1 AND 1A FOR THE DURATION OF WORK IN THIS SECTION.

STANDARD NOTES:

- ALL EXISTING UNDERGROUND UTILITIES TO BE FIELD LOCATED BY THE CONTRACTOR PRIOR TO EXCAVATION.
- ANY EXISTING UTILITIES DESTROYED DURING CONSTRUCTION ARE TO BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
- COORDINATES ARE PROVIDED FOR CONVENIENCE ONLY. ALL DIMENSIONS ON THE DRAWING TO BE VERIFIED BY CONTRACTOR PRIOR TO CONSTRUCTION.
- THE ENGINEER IS TO BE NOTIFIED OF ANY CONFLICT BETWEEN COORDINATES AND DIMENSIONS.

LEGEND:

	DAYTIME MILLING / PAVING		LOW BARRICADES C/W RED LIGHTS
	DAYTIME EXCAVATION / GRAVELS / CONCRETE PANELS		TEMPORARY 1.8m HIGH SECURE FENCING C/W RED LIGHTS
	DENOTES AIRCRAFT STAND CLOSED 24 / 7 UNTIL SECTION / PHASE IS COMPLETE AND RETURNED TO SERVICE		ORANGE REFLECTIVE DELINEATORS
	CONTRACTOR VEHICLE ROUTE		RESTRICTED AREA
	AIRCRAFT ROUTE		

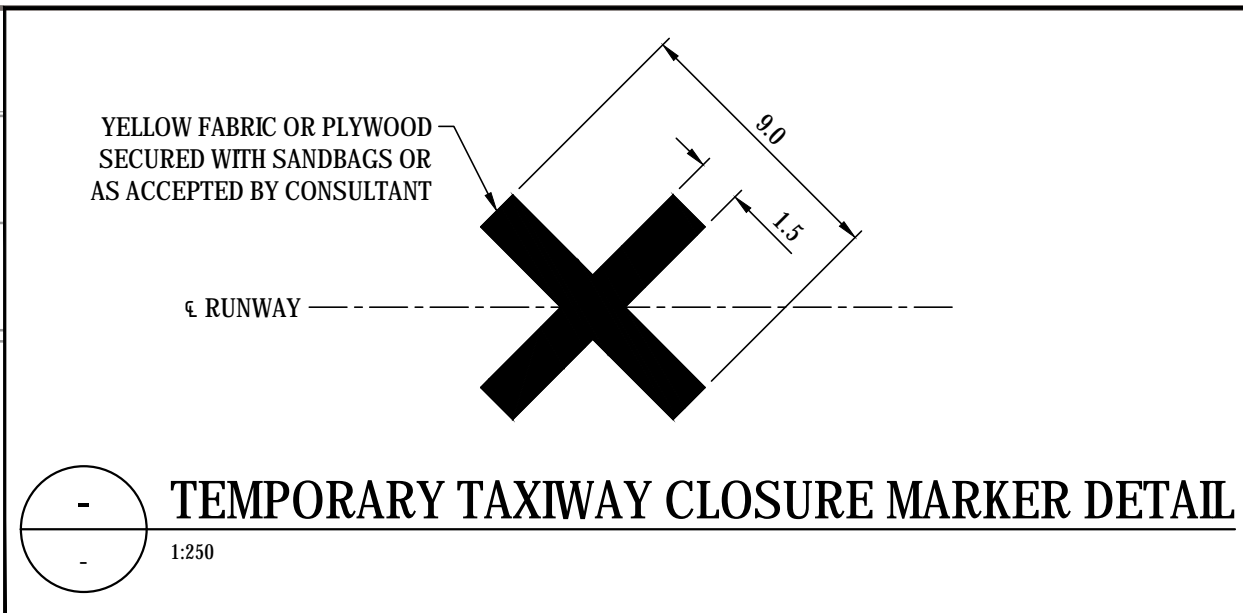
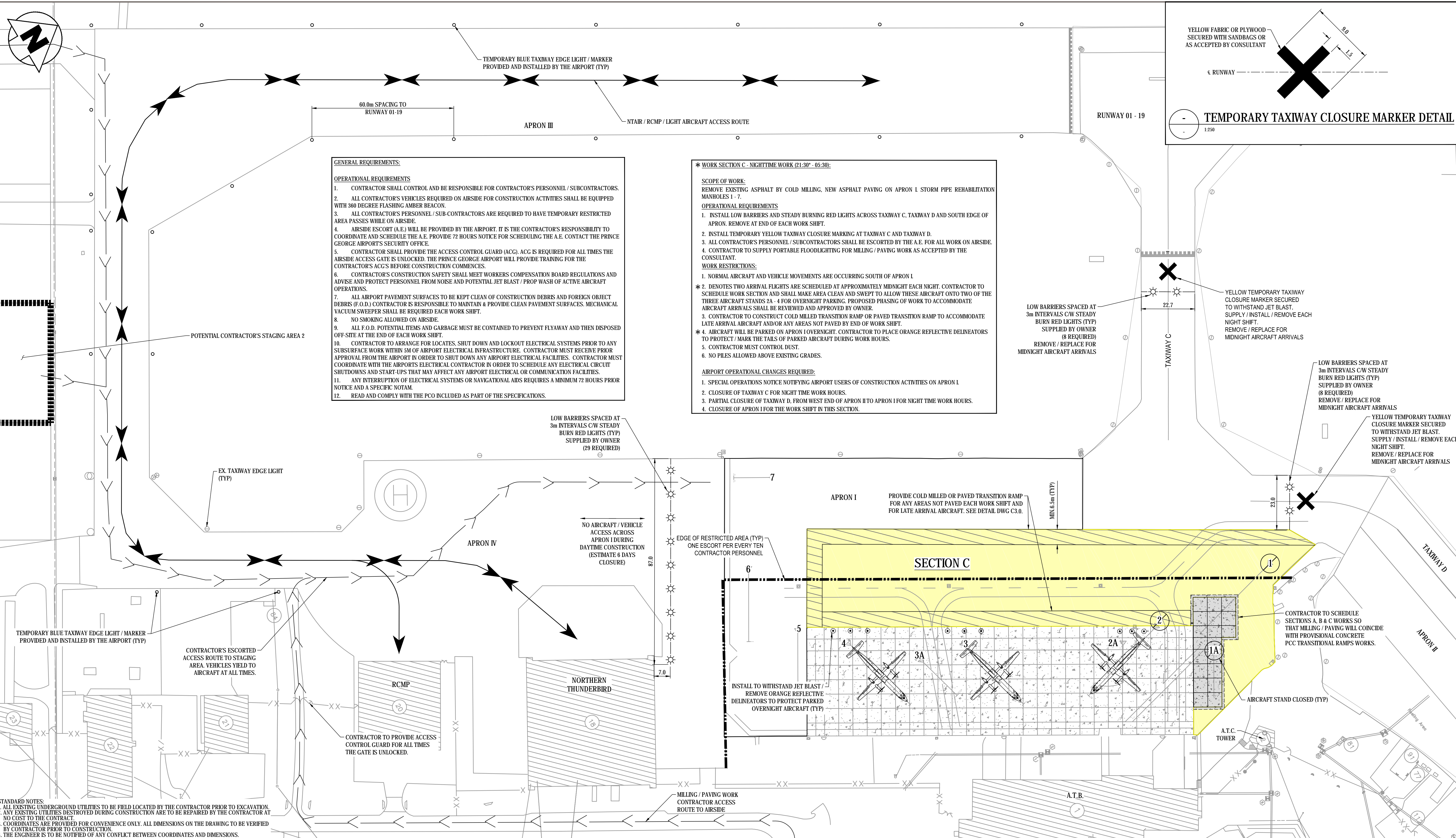


NUM	DATE	DWN	CKD	APR	DESCRIPTION
0	16/05/25				R.M.B. ISSUED FOR TENDER
					APR DESCRIPTION

REVISIONS

		REHABILITATION OF APRON I	
		PLAN OF CONSTRUCTION OPERATIONS DAY TIME WORK	
PROJECT No. 704-TRN AIRP03038	OFFICE Kelowna	DES KSA	CKD KSA
DATE April, 2016	SHEET No. 2 of 10	DWN MER	APP RMB
PERMIT		PROFESSIONAL SEAL	
		C2.0	

K:\DATA\AIRP03038 - YXS PRINCE GEORGE AIRPORT\DRAWINGS\PCO\03038_PCO_DAYTIME.dwg (22.8) May 24, 2016 - 3:02:52 pm (P.V. RUDNICKI, LMK)



- GENERAL REQUIREMENTS:**
- OPERATIONAL REQUIREMENTS**
- CONTRACTOR SHALL CONTROL AND BE RESPONSIBLE FOR CONTRACTOR'S PERSONNEL / SUBCONTRACTORS.
 - ALL CONTRACTOR'S VEHICLES REQUIRED ON AIRSIDE FOR CONSTRUCTION ACTIVITIES SHALL BE EQUIPPED WITH 360 DEGREE FLASHING AMBER BEACON.
 - ALL CONTRACTOR'S PERSONNEL / SUB-CONTRACTORS ARE REQUIRED TO HAVE TEMPORARY RESTRICTED AREA PASSES WHILE ON AIRSIDE.
 - AIRSIDE ESCORT (A.E.) WILL BE PROVIDED BY THE AIRPORT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE AND SCHEDULE THE A.E. PROVIDE 72 HOURS NOTICE FOR SCHEDULING THE A.E. CONTACT THE PRINCE GEORGE AIRPORT'S SECURITY OFFICE.
 - CONTRACTOR SHALL PROVIDE THE ACCESS CONTROL GUARD (ACG). ACG IS REQUIRED FOR ALL TIMES THE AIRSIDE ACCESS GATE IS UNLOCKED. THE PRINCE GEORGE AIRPORT WILL PROVIDE TRAINING FOR THE CONTRACTOR'S ACG BEFORE CONSTRUCTION COMMENCES.
 - CONTRACTOR'S CONSTRUCTION SAFETY SHALL MEET WORKERS COMPENSATION BOARD REGULATIONS AND ADVISE AND PROTECT PERSONNEL FROM NOISE AND POTENTIAL JET BLAST / PROP WASH OF ACTIVE AIRCRAFT OPERATIONS.
 - ALL AIRPORT PAVEMENT SURFACES TO BE KEPT CLEAN OF CONSTRUCTION DEBRIS AND FOREIGN OBJECT DEBRIS (F.O.D.) CONTRACTOR IS RESPONSIBLE TO MAINTAIN & PROVIDE CLEAN PAVEMENT SURFACES. MECHANICAL VACUUM SWEEPER SHALL BE REQUIRED EACH WORK SHIFT.
 - NO SMOKING ALLOWED ON AIRSIDE.
 - ALL F.O.D. POTENTIAL ITEMS AND GARBAGE MUST BE CONTAINED TO PREVENT FLYAWAY AND THEN DISPOSED OFF-SITE AT THE END OF EACH WORK SHIFT.
 - CONTRACTOR TO ARRANGE FOR LOCATES, SHUT DOWN AND LOCKOUT ELECTRICAL SYSTEMS PRIOR TO ANY SUBSURFACE WORK WITHIN 5M OF AIRPORT ELECTRICAL INFRASTRUCTURE. CONTRACTOR MUST RECEIVE PRIOR APPROVAL FROM THE AIRPORT IN ORDER TO SHUT DOWN ANY AIRPORT ELECTRICAL FACILITIES. CONTRACTOR MUST COORDINATE WITH THE AIRPORT'S ELECTRICAL CONTRACTOR IN ORDER TO SCHEDULE ANY ELECTRICAL CIRCUIT SHUTDOWNS AND START-UPS THAT MAY AFFECT ANY AIRPORT ELECTRICAL OR COMMUNICATION FACILITIES.
 - ANY INTERRUPTION OF ELECTRICAL SYSTEMS OR NAVIGATIONAL AIDS REQUIRES A MINIMUM 72 HOURS PRIOR NOTICE AND A SPECIFIC NOTAM.
 - READ AND COMPLY WITH THE PCO INCLUDED AS PART OF THE SPECIFICATIONS.

- * WORK SECTION C - NIGHTTIME WORK (21:30 - 05:30):**
- SCOPE OF WORK:**
REMOVE EXISTING ASPHALT BY COLD MILLING, NEW ASPHALT PAVING ON APRON I STORM PIPE REHABILITATION MANHOLES 1 - 7.
- OPERATIONAL REQUIREMENTS**
- INSTALL LOW BARRIERS AND STEADY BURNING RED LIGHTS ACROSS TAXIWAY C, TAXIWAY D AND SOUTH EDGE OF APRON. REMOVE AT END OF EACH WORK SHIFT.
 - INSTALL TEMPORARY YELLOW TAXIWAY CLOSURE MARKING AT TAXIWAY C AND TAXIWAY D.
 - ALL CONTRACTOR'S PERSONNEL / SUBCONTRACTORS SHALL BE ESCORTED BY THE A.E. FOR ALL WORK ON AIRSIDE.
 - CONTRACTOR TO SUPPLY PORTABLE FLOODLIGHTING FOR MILLING / PAVING WORK AS ACCEPTED BY THE CONSULTANT.
- WORK RESTRICTIONS:**
- NORMAL AIRCRAFT AND VEHICLE MOVEMENTS ARE OCCURRING SOUTH OF APRON I
 - DENOTES TWO ARRIVAL FLIGHTS ARE SCHEDULED AT APPROXIMATELY MIDNIGHT EACH NIGHT. CONTRACTOR TO SCHEDULE WORK SECTION AND SHALL MAKE AREA CLEAN AND SWEEP TO ALLOW THESE AIRCRAFT ONTO TWO OF THE THREE AIRCRAFT STANDS 2A - 4 FOR OVERNIGHT PARKING. PROPOSED PHASING OF WORK TO ACCOMMODATE LATE ARRIVAL AIRCRAFT AND/OR ANY AREAS NOT PAVED BY END OF WORK SHIFT.
 - CONTRACTOR TO CONSTRUCT COLD MILLED TRANSITION RAMP OR PAVED TRANSITION RAMP TO ACCOMMODATE LATE ARRIVAL AIRCRAFT AND/OR ANY AREAS NOT PAVED BY END OF WORK SHIFT.
 - AIRCRAFT WILL BE PARKED ON APRON I OVERNIGHT. CONTRACTOR TO PLACE ORANGE REFLECTIVE DELINEATORS TO PROTECT / MARK THE TAILS OF PARKED AIRCRAFT DURING WORK HOURS.
 - CONTRACTOR MUST CONTROL DUST.
 - NO PILES ALLOWED ABOVE EXISTING GRADES.
- AIRPORT OPERATIONAL CHANGES REQUIRED:**
- SPECIAL OPERATIONS NOTICE NOTIFYING AIRPORT USERS OF CONSTRUCTION ACTIVITIES ON APRON I
 - CLOSURE OF TAXIWAY C FOR NIGHT TIME WORK HOURS.
 - PARTIAL CLOSURE OF TAXIWAY D, FROM WEST END OF APRON II TO APRON I FOR NIGHT TIME WORK HOURS.
 - CLOSURE OF APRON I FOR THE WORK SHIFT IN THIS SECTION.

- STANDARD NOTES:**
- ALL EXISTING UNDERGROUND UTILITIES TO BE FIELD LOCATED BY THE CONTRACTOR PRIOR TO EXCAVATION.
 - ANY EXISTING UTILITIES DESTROYED DURING CONSTRUCTION ARE TO BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
 - COORDINATES ARE PROVIDED FOR CONVENIENCE ONLY. ALL DIMENSIONS ON THE DRAWING TO BE VERIFIED BY CONTRACTOR PRIOR TO CONSTRUCTION.
 - THE ENGINEER IS TO BE NOTIFIED OF ANY CONFLICT BETWEEN COORDINATES AND DIMENSIONS.

LEGEND:

- NIGHTTIME MILLING PAVING, STORM REHABILITATION
- DENOTES AIRCRAFT STAND CLOSED 24 / 7 UNTIL SECTION / PHASE IS COMPLETE AND RETURNED TO SERVICE
- CONTRACTOR VEHICLE ROUTE
- AIRCRAFT ROUTE
- LOW BARRICADES C/W RED LIGHTS
- TEMPORARY 1.8m HIGH SECURE FENCING C/W RED LIGHTS
- ORANGE REFLECTIVE DELINEATORS
- RESTRICTED AREA

Scale: 1:750 @ 22"x34"

NUM	DATE	DWN	CKD	APR	DESCRIPTION
0	16/05/25				R.M.B. ISSUED FOR TENDER

NUM	DATE	APR	DESCRIPTION

CLIENT: **YXS** prince george airport authority

TETRA TECH

REHABILITATION OF APRON I

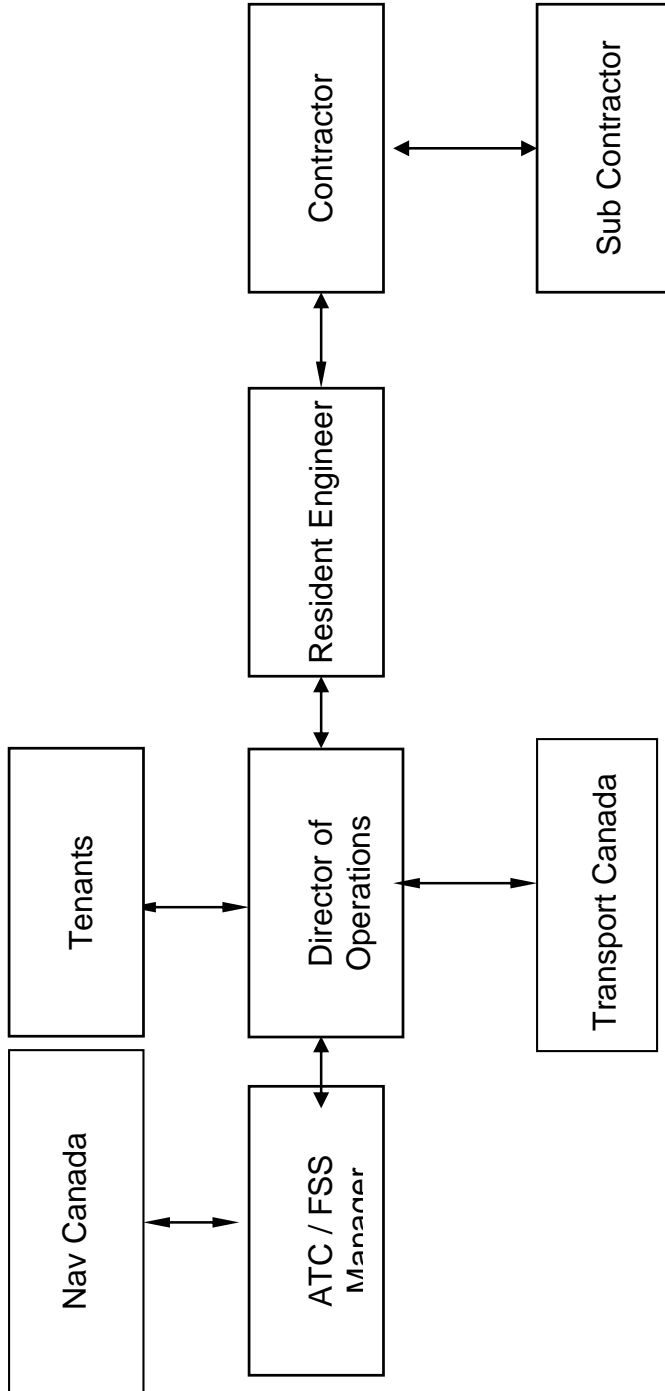
PLAN OF CONSTRUCTION OPERATIONS NIGHT TIME WORK

PROJECT No.	OFFICE	DES	CKD	REV	DRAWING
704-TRN AIRP03038	Kelowna	KSA	KSA		
DATE:	SHEET No.	DWN	APP	STATUS	
April, 2016	3 of 10	MER	RMB	0	C2.1

PERMIT: _____ PROFESSIONAL SEAL: _____

ACTIVITY: AIRP03038 - YXS PRINCE GEORGE AIRPORT AUTHORITY - YXS C2.1-PCO NIGHT TIME WORK (C2.1) - May 25, 2016 - 6:48:24 am (BY: SUDHISHK MINE)

Annex 2



LINES OF COMMUNICATION
FOR
Rehabilitation of Apron I

AT PRINCE GEORGE AIRPORT, BC

Contacts Phone Numbers

Name	Office	Cell	Fax	Email
Contractor (TBD)				

Prince George Airport (YXS)				
Director of Operations – Cuyler Green	(250) 963-2414			cgreen@pgairport.ca
Airport Operations – Trevor Gust		(250) 612-2493		tgust@pgairport.ca

Tetra Tech EBA Inc.	(250)862-4832		(250) 862-2941	
Project Manager - Kevin Alexander	Ext 226	(250) 718-9665		kevin.alexander@tetrattech.com
Resident Engineer – Gabriel Francis		(403) 869-5017		gabriel.francis@tetrattech.com

EMERGENCIES – CALL 911
(ATC and YXS Operations must also be notified by Contractor / Airside Escort)

Annex 3

Contractor Airport Orientation

Geographic Orientation

Attached you will find a map of the Prince George Airport site. You should learn everything from it, with the following things in mind:

Boundaries

Two words that are new to you are "groundside" and "airside". These are self-explanatory and, at any airport, these two sides are well marked and physically divided by fences. On your airport map find the fences and note that they enclose the airside, the area used only for "activities directly related to the aircraft operation". Anything outside this area but still on the airport site is referred to as "groundside". Boundary and security functions of all restricted entrances to the airside must work together to prevent everything and everyone from animals and children to armed hijackers from gaining entry.

Security Gates

Gates are kept locked for security reasons and, unless manned by a Access Control person, the gate must be closed and locked behind you when you use it, no matter how soon you expect to return.

Airport Movement Areas

Runways

Runways are used for aircraft takeoffs and landings. If you add a "0" to the runway number, you will get its magnetic direction to the nearest 10° (i.e., Runway 16-34 has magnetic bearing 160° or 340°). You will not be near any runways on this project.

Taxiways

Taxiways are for aircraft movements other than takeoffs and landings. For a portion of the work, you will be working up to the edge on Taxiway Charlie and Delta. You must stay within the designated work section unless given prior permission by the Airside Escort, and escorted by the Airside Escort.

Aprons

Aprons are for aircraft parking, loading and fuelling. You will be working on Apron I (it will be active or closed depending on what Work Section you are working in. You must stay inside the work section unless given prior permission by the Airside Escort, and escorted by the Airside Escort.

Identification of Taxiways and Aprons

When you speak of an apron, you use a number (Roman numerals), such as Apron I (pronounced Apron Three).

When you speak of a taxiway you use a letter, such as Taxiway "D". The letter is displayed on a sign to identify the particular taxiway. The taxiways are actually called Alpha, Bravo, Charlie, Delta and Echo because they are referred to by the phonetic alphabet.

Barricades

All workers must remain within barricades /fencing on the airside of this project. Stay within sight of the Airside Escort at all times, you must be escorted for all your movements on airside. **DO NOT LEAVE THE IMMEDIATE WORK AREA AS YOU ARE ABOUT TO ENTER A RESTRICTED AREA.**

Vehicle Traffic Regulations

All drivers of vehicles airside must hold a valid driver's licence issued by the Province of British Columbia. When on and off airport property, you are required to observe all municipal, provincial and federal traffic regulations.

Other Regulations

NO SMOKING REGULATIONS ARE IN EFFECT IN ALL CONSTRUCTION AREAS.

FAILURE TO OBEY THE INSTRUCTIONS CONTAINED IN THIS FORM WILL RESULT IN THE PERSONS INVOLVED BEING REMOVED FROM THE WORK SITE.

I have reviewed the "Contractor Airport Orientation" and understand the contents of this form.

Foreman _____ Date _____

Employee _____ Date _____

ICAO Phonetic Alphabet and Pronunciation of Numbers

Always use the ICAO Phonetic Alphabet when phonetics are required for clarity in radiotelephone communications.

Letter	Word	Spoken as
A	ALFA	(Al fah)
B	BRAVO	(BRAH VOH)
C	CHARLIE	(CHAR lee)
D	DELTA	(DELL tah)
E	ECHO	(ECK oh)
F	FOXTROT	(TOKS trot)
G	GOLF	(GOLF)
H	HOTEL	(hoh TELL)
I	INDIA	(IN dee ah)
J	JULIET	(JEW lee ETT)
K	KILO	(KEY loh)
L	LIMA	(LEE mah)
M	MIKE	(MIKE)
N	NOVEMBER	(no VEM ber)
O	OSCAR	(OSS cah)
P	PAPA	(pah PAH)
Q	QUEBEC	(keh BECK)
R	ROMEO	(ROW me oh)
S	SIERRA	(see AIR rah)
T	TANGO	(TANG go)
U	UNIFORM	(YOU nee form)
V	VICTOR	(VIK tah)
W	WHISKEY	(WISS key)
X	X-RAY	(ECKS ray)
Y	YANKEE	(YANG key)
Z	ZULU	(ZOO loo)

Pronounce numbers as:

0	ZE-RO	5	FIFE
1	WUN	6	SIX
2	TOO	7	SEV-en
3	TREE	8	AIT
4	FOW-er	9	NIN-er

Note: Stress the syllables printed in CAPITAL letters. For example, give the two syllables in ZE-RO equal emphasis, but give the first syllable for FOW-er primary emphasis.

Annex 4

Flight Schedule

PRINCE GEORGE AIRPORT FLIGHT SCHEDULE - EFFECTIVE MAY 1 TO 9, 2016

Flight Time	Arrival/Departure	SUN	MON	TUE	WED	THU	FRI	SAT	Route	Aircraft
0600	Departure	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	Vancouver (YVR)	DH8/4
0630	Departure	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	Vancouver (YVR)	DH4
0840	Arrival		GLR721	GLR721	GLR721	GLR721			Kamloops (YKA)	1900
0840	Arrival		GLR725	GLR725	GLR725	GLR725			Kelowna (YLW)	DH1
0905	Departure		GLR743	GLR743	GLR743	GLR743			Ft St John (YXJ)	1900
0905	Arrival	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	Vancouver (YVR)	DH8/4
0930	Departure		GLR711	GLR711	GLR711	GLR711			Terrace (YXT)	DH1
0935	Departure	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	Vancouver (YVR)	DH8/4
0945	Departure		GLR701	GLR701	GLR701				Smithers (YYD)	1900
0955	Arrival			GLR201	GLR201				Vancouver (YVR)	Dornier
1012	Arrival	WS3283	WS3283	WS3283	WS3283	WS3283	WS3283		Vancouver (YVR)	DH4
1020	Departure			GLR201	GLR201				Dawson Creek (YDQ)	Dornier
1045	Departure	WS3282	WS3282	WS3282	WS3282	WS3282	WS3282		Vancouver (YVR)	DH4
1105	Arrival		GLR751	GLR751	GLR751	GLR751			Vancouver (YVR)	Dornier
1130	Arrival	GLR730					GLR730		Kelowna (YLW)	DH1
1130	Departure		GLR881	GLR881	GLR881	GLR881			Ft Nelson (YYE)	Dornier
1140	Arrival	GLR727					GLR727		Kamloops (YKA)	1900
1155	Arrival	GLR753					GLR753		Vancouver (YVR)	Dornier
1155	Departure						GLR205		Ft St John (YXJ)	DH8
1205	Arrival		PC1461	PC1461	PC1461	PC1461	PC1461		Victoria (YYJ)	SAAB 340A
1215	Departure	GLR717					GLR717		Terrace (YXT)	DH1
1225	Departure	GLR883					GLR883		Ft Nelson (YYE)	Dornier
1230	Departure	GLR700				GLR700	GLR700		Smithers (YYD)	1900
1230	Departure		PC1414	PC1414	PC1414	PC1414	PC1414		Victoria (YYJ)	SAAB 340A
1240	Departure	GLR747							Ft St John (YXJ)	1900
1310	Arrival		GLR886	GLR886	GLR886				Ft Nelson (YYE)	1900
1340	Arrival		GLR702	GLR702	GLR702				Smithers (YYD)	1900
1345	Arrival	AC8205	AC8205	AC8205	AC8205	AC8205	AC8205		Vancouver (YVR)	DH8/4
1410	Departure		GLR756	GLR756	GLR756				Vancouver (YVR)	Dornier
1415	Departure	AC8208	AC8208	AC8208	AC8208	AC8208	AC8208		Vancouver (YVR)	DH8/4
1427	Arrival	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	Vancouver (YVR)	DH4
1505	Arrival	PC1435							Victoria (YYJ)	SAAB 340A
1505	Departure	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	Vancouver (YVR)	DH4
1510	Arrival	GLR888				GLR888	GLR888		Ft Nelson (YYE)	1900
1530	Departure	PC1466							Victoria (YYJ)	SAAB 340A
1545	Arrival	GLR704				GLR704	GLR704		Smithers (YYD)	1900
1550	Arrival	GLR749	GLR748	GLR748	GLR748	GLR748			Ft St John (YXJ)	1900
1550	Arrival						GLR205		Dawson Creek (YDQ)	DH8
1555	Arrival	GLR718	GLR716	GLR716	GLR716	GLR716	GLR716		Terrace (YXT)	DH1
1620	Departure	GLR758				GLR758	GLR758		Vancouver (YVR)	DH1
1635	Departure	GLR722	GLR722	GLR722	GLR722	GLR722	GLR722		Kamloops (YKA)	1900
1650	Departure	GLR728	GLR728	GLR728	GLR728	GLR728	GLR728		Kelowna (YLW)	DH1
1650	Arrival		AC8209	AC8209	AC8209	AC8209	AC8209		Vancouver (YVR)	DH8/4
1706	Arrival							AC8209	Vancouver (YVR)	DH8/3
1720	Departure		AC8212	AC8212	AC8212	AC8212	AC8212		Vancouver (YVR)	DH8/4
1730	Departure							AC8212	Vancouver (YVR)	DH8/3
1810	Arrival		PC1417	PC1417	PC1417	PC1417	PC1417		Victoria (YYJ)	SAAB 340A
1817	Arrival	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	Vancouver (YVR)	DH4
1835	Departure		PC1418	PC1418	PC1418	PC1418	PC1418		Victoria (YYJ)	SAAB 340A
1855	Departure	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	Vancouver (YVR)	DH4
2025	Arrival	AC8209	AC8211	AC8211	AC8211	AC8211	AC8211		Vancouver (YVR)	DH8/4
2055	Departure	AC8214	AC8214	AC8214	AC8214	AC8214	AC8214		Vancouver (YVR)	DH8/4
2247	Arrival	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	Vancouver (YVR)	DH4
2359	Arrival	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	Vancouver (YVR)	DH8/4

AC= Jazz

GLR = CMA

PC= Pacific Coastal

WS Mexico

PRINCE GEORGE AIRPORT FLIGHT SCHEDULE - EFFECTIVE MAY 10 TO 17, 2016

Flight Time	Arrival/Departure	TUE	WED	THU	FRI	SAT	SUN	MON	Route	Aircraft
0600	Departure	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	Vancouver (YVR)	DH8/4
0630	Departure	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	Vancouver (YVR)	DH4
0840	Arrival	GLR721	GLR721	GLR721				GLR721	Kamloops (YKA)	1900
0840	Arrival	GLR725	GLR725	GLR725				GLR725	Kelowna (YLW)	DH1
0905	Departure	GLR743	GLR743	GLR743				GLR743	Ft St John (YXJ)	1900
0905	Arrival	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	Vancouver (YVR)	DH8/4
0930	Departure	GLR711	GLR711	GLR711				GLR711	Terrace (YXT)	DH1
0935	Departure	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	Vancouver (YVR)	DH8/4
0945	Departure	GLR701	GLR701					GLR701	Smithers (YYD)	1900
1012	Arrival	WS3283	WS3283	WS3283	WS3283		WS3283	WS3283	Vancouver (YVR)	DH4
1045	Departure	WS3282	WS3282	WS3282	WS3282		WS3282	WS3282	Vancouver (YVR)	DH4
1105	Arrival	GLR751	GLR751	GLR751				GLR751	Vancouver (YVR)	Dornier
1130	Arrival				GLR730		GLR730		Kelowna (YLW)	DH1
1130	Departure	GLR881	GLR881	GLR881				GLR881	Ft Nelson (YYE)	Dornier
1140	Arrival				GLR727		GLR727		Kamloops (YKA)	1900
1155	Arrival				GLR753		GLR753		Vancouver (YVR)	Dornier
1205	Arrival	PC1461	PC1461	PC1461	PC1461			PC1461	Victoria (YYJ)	SAAB 340A
1215	Departure				GLR717		GLR717		Terrace (YXT)	DH1
1225	Departure				GLR883		GLR883		Ft Nelson (YYE)	Dornier
1230	Departure			GLR700	GLR700		GLR700		Smithers (YYD)	1900
1230	Departure	PC1414	PC1414	PC1414	PC1414			PC1414	Victoria (YYJ)	SAAB 340A
1240	Departure				GLR747		GLR747		Ft St John (YXJ)	1900
1310	Arrival	GLR886	GLR886					GLR886	Ft Nelson (YYE)	1900
1340	Arrival	GLR702	GLR702					GLR702	Smithers (YYD)	1900
1345	Arrival	AC8205	AC8205	AC8205	AC8205		AC8205	AC8205	Vancouver (YVR)	DH8/4
1410	Departure	GLR756	GLR756					GLR756	Vancouver (YVR)	Dornier
1415	Departure	AC8208	AC8208	AC8208	AC8208		AC8208	AC8208	Vancouver (YVR)	DH8/4
1427	Arrival	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	Vancouver (YVR)	DH4
1505	Arrival						PC1435		Victoria (YYJ)	SAAB 340A
1505	Departure	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	Vancouver (YVR)	DH4
1510	Arrival			GLR888	GLR888		GLR888		Ft Nelson (YYE)	1900
1530	Departure						PC1466		Victoria (YYJ)	SAAB 340A
1545	Arrival			GLR704	GLR704		GLR704		Smithers (YYD)	1900
1550	Arrival	GLR748	GLR748	GLR748	GLR748		GLR749	GLR748	Ft St John (YXJ)	1900
1555	Arrival	GLR716	GLR716	GLR716	GLR716		GLR718	GLR716	Terrace (YXT)	DH1
1620	Departure			GLR758	GLR758		GLR758		Vancouver (YVR)	DH1
1635	Departure	GLR722	GLR722	GLR722	GLR722		GLR722	GLR722	Kamloops (YKA)	1900
1650	Departure	GLR728	GLR728	GLR728	GLR728		GLR728	GLR728	Kelowna (YLW)	DH1
1650	Arrival	AC8209	AC8209	AC8209	AC8209			AC8209	Vancouver (YVR)	DH8/4
1706	Arrival					AC8209			Vancouver (YVR)	DH8/3
1720	Departure	AC8212	AC8212	AC8212	AC8212			AC8212	Vancouver (YVR)	DH8/4
1730	Departure					AC8212			Vancouver (YVR)	DH8/3
1810	Arrival	PC1417	PC1417	PC1417	PC1417			PC1417	Victoria (YYJ)	SAAB 340A
1817	Arrival	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	Vancouver (YVR)	DH4
1835	Departure	PC1418	PC1418	PC1418	PC1418			PC1418	Victoria (YYJ)	SAAB 340A
1855	Departure	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	Vancouver (YVR)	DH4
2025	Arrival	AC8211	AC8211	AC8211	AC8211		AC8209	AC8211	Vancouver (YVR)	DH8/4
2055	Departure	AC8214	AC8214	AC8214	AC8214		AC8214	AC8214	Vancouver (YVR)	DH8/4
2247	Arrival	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	Vancouver (YVR)	DH4
2359	Arrival	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	Vancouver (YVR)	DH8/4

AC= Jazz

GLR = CMA

PC= Pacific Coastal

WS Mexico

PRINCE GEORGE AIRPORT FLIGHT SCHEDULE - EFFECTIVE MAY 18 TO 24, 2016

Flight Time	Arrival/Departure	WED 18	THU 19	FRI 20	SAT 21	SUN 22	MON 23	TUE 24	Route	Aircraft
0600	Departure	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	Vancouver (YVR)	DH8/4
0630	Departure	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	Vancouver (YVR)	DH4
0840	Arrival	GLR721	GLR721					GLR721	Kamloops (YKA)	1900
0840	Arrival	GLR725	GLR725					GLR725	Kelowna (YLW)	DH1
0850	Arrival	GLR755							Vancouver (YVR)	Dornier
0905	Departure	GLR743	GLR743					GLR743	Ft St John (YXJ)	1900
0905	Arrival	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	Vancouver (YVR)	DH8/4
0915	Departure	GLR755							Ft Nelson (YYE)	Dornier
0930	Departure	GLR711	GLR711					GLR711	Terrace (YXT)	DH1
0935	Departure	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	Vancouver (YVR)	DH8/4
0945	Departure	GLR701						GLR701	Smithers (YYD)	1900
1012	Arrival	WS3283	WS3283	WS3283		WS3283	WS3283	WS3283	Vancouver (YVR)	DH4
1045	Departure	WS3282	WS3282	WS3282		WS3282	WS3282	WS3282	Vancouver (YVR)	DH4
1105	Arrival		GLR751					GLR751	Vancouver (YVR)	Dornier
1130	Arrival			GLR730					Kelowna (YLW)	DH1
1130	Departure		GLR881					GLR881	Ft Nelson (YYE)	Dornier
1140	Arrival			GLR727			GLR720		Kamloops (YKA)	1900
1155	Arrival			GLR753			GLR753		Vancouver (YVR)	Dornier
1155	Arrival						GLR706		Smithers (YYD)	1900
1205	Arrival	PC1461	PC1461	PC1461			PC1461	PC1461	Victoria (YYJ)	SAAB 340A
1215	Departure			GLR717			GLR717		Terrace (YXT)	DH1
1225	Departure			GLR883			GLR883		Ft Nelson (YYE)	Dornier
1230	Departure		GLR700	GLR700			GLR700		Smithers (YYD)	1900
1230	Departure	PC1414	PC1414	PC1414			PC1414	PC1414	Victoria (YYJ)	SAAB 340A
1240	Departure			GLR747			GLR747		Ft St John (YXJ)	1900
1310	Arrival	GLR886						GLR886	Ft Nelson (YYE)	1900
1340	Arrival	GLR702						GLR702	Smithers (YYD)	1900
1345	Arrival	AC8205	AC8205	AC8205		AC8205	AC8205	AC8205	Vancouver (YVR)	DH8/4
1410	Departure	GLR756						GLR756	Vancouver (YVR)	Dornier
1415	Departure	AC8208	AC8208	AC8208		AC8208	AC8208	AC8208	Vancouver (YVR)	DH8/4
1427	Arrival	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	Vancouver (YVR)	DH4
1505	Arrival					PC1435			Victoria (YYJ)	SAAB 340A
1505	Departure	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	Vancouver (YVR)	DH4
1510	Arrival		GLR888	GLR888			GLR888		Ft Nelson (YYE)	1900
1530	Departure					PC1466			Victoria (YYJ)	SAAB 340A
1545	Arrival		GLR704	GLR704			GLR704		Smithers (YYD)	1900
1550	Arrival	GLR748	GLR748	GLR748			GLR748	GLR748	Ft St John (YXJ)	1900
1555	Arrival	GLR716	GLR716	GLR716			GLR716	GLR716	Terrace (YXT)	DH1
1620	Departure		GLR758	GLR758			GLR758		Vancouver (YVR)	DH1
1635	Departure	GLR722	GLR722	GLR722			GLR722	GLR722	Kamloops (YKA)	1900
1650	Departure	GLR728	GLR728	GLR728			GLR728	GLR728	Kelowna (YLW)	DH1
1650	Arrival	AC8209	AC8209	AC8209			AC8209	AC8209	Vancouver (YVR)	DH8/4
1705	Departure			GLR705					Smithers (YYD)	1900
1706	Arrival				AC8209				Vancouver (YVR)	DH8/3
1720	Departure	AC8212	AC8212	AC8212			AC8212	AC8212	Vancouver (YVR)	DH8/4
1730	Departure				AC8212				Vancouver (YVR)	DH8/3
1810	Arrival	PC1417	PC1417	PC1417			PC1417	PC1417	Victoria (YYJ)	SAAB 340A
1817	Arrival	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	Vancouver (YVR)	DH4
1835	Departure	PC1418	PC1418	PC1418			PC1418	PC1418	Victoria (YYJ)	SAAB 340A
1855	Departure	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	Vancouver (YVR)	DH4
1915	Arrival			GLR723					Kamloops (YKA)	1900
2025	Arrival	AC8211	AC8211	AC8211		AC8209	AC8211	AC8211	Vancouver (YVR)	DH8/4
2055	Departure	AC8214	AC8214	AC8214		AC8214	AC8214	AC8214	Vancouver (YVR)	DH8/4
2247	Arrival	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	Vancouver (YVR)	DH4
2359	Arrival	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	Vancouver (YVR)	DH8/4

AC= Jazz

GLR = CMA

PC= Pacific Coastal

WS Mexico

PRINCE GEORGE AIRPORT FLIGHT SCHEDULE - EFFECTIVE MAY 25 TO 31, 2016

Flight Time	Arrival/Departure	WED 25	THU 26	FRI 27	SAT 28	SUN 29	MON 30	TUE 31	Route	Aircraft
0600	Departure	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	AC8202	Vancouver (YVR)	DH8/4
0630	Departure	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	WS3290	Vancouver (YVR)	DH4
0840	Arrival	GLR721	GLR721				GLR721	GLR721	Kamloops (YKA)	1900
0840	Arrival	GLR725	GLR725				GLR725	GLR725	Kelowna (YLW)	DH1
0905	Departure	GLR743	GLR743				GLR743	GLR743	Ft St John (YXJ)	1900
0905	Arrival	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	AC8201	Vancouver (YVR)	DH8/4
0930	Departure	GLR711	GLR711				GLR711	GLR711	Terrace (YXT)	DH1
0935	Departure	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	AC8204	Vancouver (YVR)	DH8/4
0945	Departure	GLR701					GLR701	GLR701	Smithers (YYD)	1900
1012	Arrival	WS3283	WS3283	WS3283		WS3283	WS3283	WS3283	Vancouver (YVR)	DH4
1045	Departure	WS3282	WS3282	WS3282		WS3282	WS3282	WS3282	Vancouver (YVR)	DH4
1105	Arrival	GLR751	GLR751				GLR751	GLR751	Vancouver (YVR)	Dornier
1130	Arrival			GLR730		GLR730			Kelowna (YLW)	DH1
1130	Departure	GLR881	GLR881				GLR881	GLR881	Ft Nelson (YYE)	Dornier
1140	Arrival			GLR727		GLR727			Kamloops (YKA)	1900
1155	Arrival			GLR753		GLR753			Vancouver (YVR)	Dornier
1205	Arrival	PC1461	PC1461	PC1461			PC1461	PC1461	Victoria (YYJ)	SAAB 340A
1215	Departure			GLR717		GLR717			Terrace (YXT)	DH1
1225	Departure			GLR883		GLR883			Ft Nelson (YYE)	Dornier
1230	Departure		GLR700	GLR700		GLR700			Smithers (YYD)	1900
1230	Departure	PC1414	PC1414	PC1414			PC1414	PC1414	Victoria (YYJ)	SAAB 340A
1240	Departure			GLR747		GLR747			Ft St John (YXJ)	1900
1310	Arrival	GLR886					GLR886	GLR886	Ft Nelson (YYE)	1900
1340	Arrival	GLR702					GLR702	GLR702	Smithers (YYD)	1900
1345	Arrival	AC8205	AC8205	AC8205		AC8205	AC8205	AC8205	Vancouver (YVR)	DH8/4
1410	Departure	GLR756					GLR756	GLR756	Vancouver (YVR)	Dornier
1415	Departure	AC8208	AC8208	AC8208		AC8208	AC8208	AC8208	Vancouver (YVR)	DH8/4
1427	Arrival	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	WS3285	Vancouver (YVR)	DH4
1505	Arrival					PC1435			Victoria (YYJ)	SAAB 340A
1505	Departure	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	WS3284	Vancouver (YVR)	DH4
1510	Arrival		GLR888	GLR888		GLR888			Ft Nelson (YYE)	1900
1530	Departure					PC1466			Victoria (YYJ)	SAAB 340A
1545	Arrival		GLR704	GLR704		GLR704			Smithers (YYD)	1900
1550	Arrival	GLR748	GLR748	GLR748		GLR749	GLR748	GLR748	Ft St John (YXJ)	1900
1555	Arrival	GLR716	GLR716	GLR716		GLR718	GLR716	GLR716	Terrace (YXT)	DH1
1620	Departure		GLR758	GLR758		GLR758			Vancouver (YVR)	DH1
1635	Departure	GLR722	GLR722	GLR722		GLR722	GLR722	GLR722	Kamloops (YKA)	1900
1650	Departure	GLR728	GLR728	GLR728		GLR728	GLR728	GLR728	Kelowna (YLW)	DH1
1650	Arrival	AC8209	AC8209	AC8209			AC8209	AC8209	Vancouver (YVR)	DH8/4
1706	Arrival				AC8209				Vancouver (YVR)	DH8/3
1720	Departure	AC8212	AC8212	AC8212			AC8212	AC8212	Vancouver (YVR)	DH8/4
1730	Departure				AC8212				Vancouver (YVR)	DH8/3
1810	Arrival	PC1417	PC1417	PC1417			PC1417	PC1417	Victoria (YYJ)	SAAB 340A
1817	Arrival	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	WS3297	Vancouver (YVR)	DH4
1835	Departure	PC1418	PC1418	PC1418			PC1418	PC1418	Victoria (YYJ)	SAAB 340A
1855	Departure	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	WS3298	Vancouver (YVR)	DH4
2025	Arrival	AC8211	AC8211	AC8211		AC8209	AC8211	AC8211	Vancouver (YVR)	DH8/4
2055	Departure	AC8214	AC8214	AC8214		AC8214	AC8214	AC8214	Vancouver (YVR)	DH8/4
2247	Arrival	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	WS3287	Vancouver (YVR)	DH4
2359	Arrival	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	AC8213	Vancouver (YVR)	DH8/4

AC= Jazz

GLR = CMA

PC= Pacific Coastal

WS Mexico

Annex 5

TP 312E 4th Edition – Article 7.4

7.4 UNSERVICEABLE AREAS

7.4.1 UNSERVICEABILITY MARKERS

Application

7.4.1.1 Standard – Unserviceability markers shall be displayed wherever any portion of a taxiway, apron or holding bay is unfit for the movement of aircraft but it is still possible for aircraft to bypass the area safely.

Note – Unserviceability markers are used for such purposes as warning pilots of a hole in a taxiway or apron pavement or outlining a portion of pavement, such as on an apron, that is under repair. They are not suitable for use when a portion of a runway becomes unserviceable, nor on a taxiway when a major portion of the width becomes unserviceable. In such instances, the runway or taxiway is normally closed.

Location

7.4.1.2 Standard – Unserviceability markers shall be placed at intervals sufficiently close so as to delineate the unserviceable area.

Characteristics

7.4.1.3 Standard – Unserviceability markers shall consist of conspicuous upstanding devices such as flags, cones, or marker boards.

7.4.1.4 Recommendation – An unserviceability cone should be at least 0.5m in height and red or international orange in combination with white.

7.4.1.5 Recommendation – An unserviceability flag should be at least 0.5m square and red or international orange in combination with white.

7.4.1.6 Recommendation – An unserviceability marker board should be at

least 0.5m in height and 0.9m in length, with alternate red and white or international orange and white vertical stripes.

7.4.2 UNSERVICEABILITY LIGHTS

Application

7.4.2.1 Standard – Unserviceability lights shall be displayed wherever any portion of a taxiway, apron or holding bay used at night is unfit for the movement of aircraft but it is still possible for aircraft to bypass the area safely.

Note – Unserviceability lights are used for such purposes as warning pilots of a hole in a taxiway or apron pavement or outlining a portion of pavement, such as on an apron, that is under repair. They are not suitable for use when a portion of a runway becomes unserviceable, nor on a taxiway when a major portion of the width becomes unserviceable. In such instances, the runway or taxiway is normally closed.

Location

7.4.2.2 Standard – Unserviceability lights shall be placed at intervals sufficiently close so as to delineate the unserviceable area.

Characteristics

7.4.2.3 Standard – An unserviceability light shall consist of a red fixed light. The red fixed light shall have intensity sufficient to ensure conspicuity considering the intensity of the adjacent lights and the general level of illumination against which it would normally be viewed. In no case shall the intensity be less than 10 cd of red light.

APPROVAL OF PLAN OF CONSTRUCTION OPERATIONS

PROJECT:

Rehabilitation of Apron I at Prince George Airport

AIRPORT NAME:

Prince George Airport

AIRPORT OPERATOR and CERTIFICATE HOLDER:

Prince George Airport Authority

DIRECTOR OF OPERATIONS:

Cuyler Green

CERTIFICATE NUMBER:

TAMB 5151-P151

DATE OF ISSUE:

December 5, 2011

I undertake to meet the obligations set out in this plan of construction; and I hereby certify that the information in this plan is complete and accurate and no relevant information has been omitted.

Date (Y-M-D)

Signature of Airport Operator/Certificate Holder

This Plan of Construction Operations Manual/Amendments is approved

Date (Y-M-D)

for Minister of Transport